



SB-9

OBJECTIVE DESIGN STANDARDS





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Updated: May 2024

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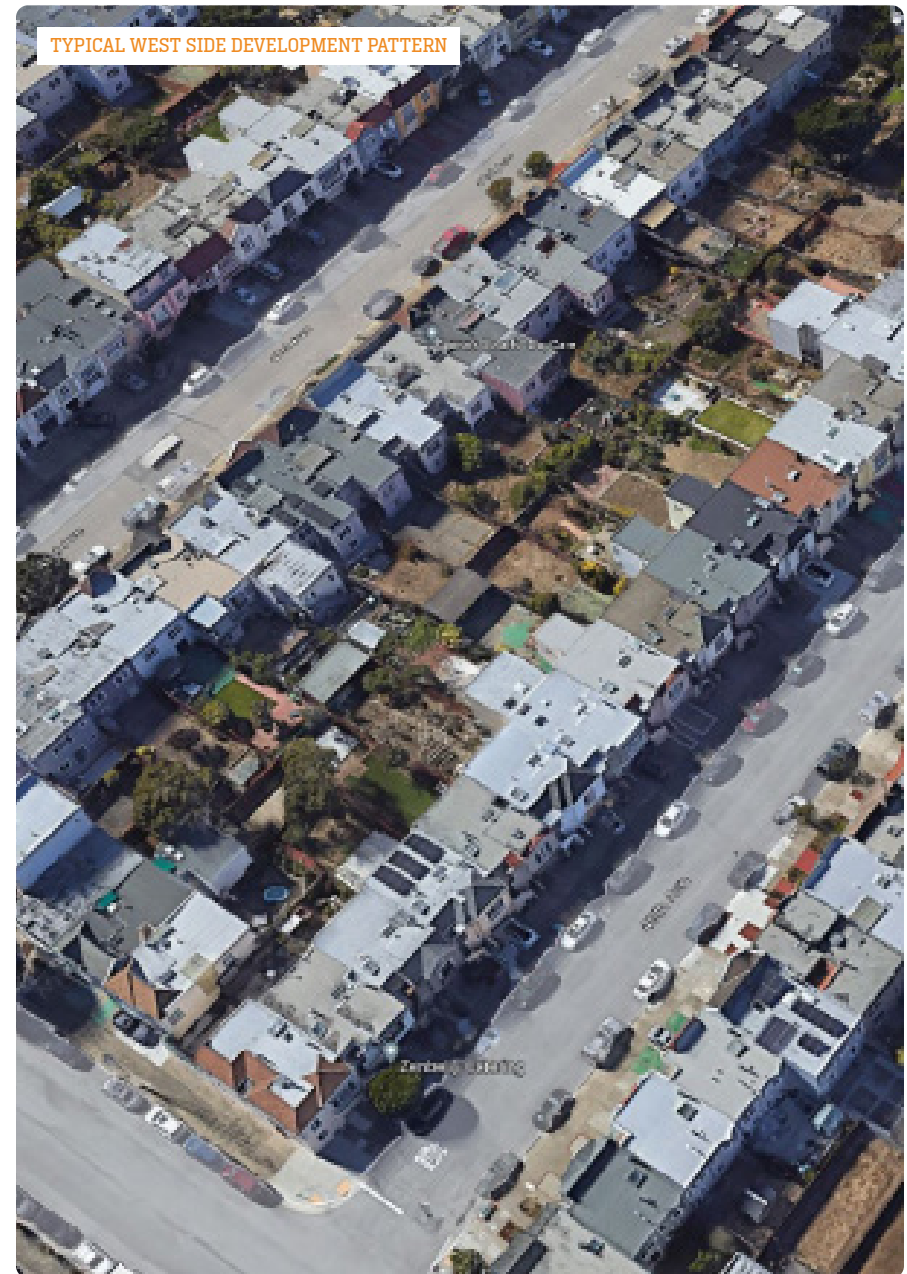
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Regulatory Context

SB-9 is a State law that went into effect January 1, 2022. It requires single-family zoning districts (RH-1) to accommodate 2 dwelling units per lot, or for single-family RH-1 lots to be split and for each to accommodate 2 dwelling units. The state law requires ministerial approval and restricts the use of discretionary review to limit the resulting development. However, local jurisdictions can enact development standards such that they do not preclude the construction of two 800 square foot dwellings per parcel.

Since the Residential Design Guidelines and certain aspects of the Planning Code would not be applicable the following development standards apply to all projects proposing to use SB-9. They are based on historical precedent, accepted and achievable standards already in the Planning Code and Residential Design Guidelines adapted to balance the goals of SB-9 with zoning controls while maintaining a high-quality residential environment.

This is not without historical precedent. Lots with residential buildings in the rear and front are a common development pattern in San Francisco stemming from rebuilding after the 1906 earthquake and fire. In many cases older houses were moved to the rear of lots to provide space to construct larger residential buildings of flats at the front of lots. In those cases, smaller buildings were located in the rear of the lots and separated by an open court that allows adequate exposure and open space for all dwellings.



SB-9 General Criteria

1

Per State law the development must not require demolition or alteration of any of the following types of housing:

- » Affordable housing for persons or families of moderate, low, or very low incomes
- » Rent-controlled housing
- » Housing which has been occupied by a tenant in the last three years

2

The development must not be located within a historic district or on a historic property site.

3

The development must not allow for the demolition of more than 25 percent of the existing exterior structural walls, unless:


- » A local ordinance so allows
- » The development site has not been occupied by a tenant in the last three years

Application of the Standards

The SB-9 Standards apply to projects seeking to add one or more residential units in RH-1 and RH-1(D) zoning districts.

Projects must meet the objective design standards and all applicable Planning Code requirements.

Guideline Structure: Each standard is described at the top of the page, followed by a sidebar that explains the rationale for the standard, and illustrations that further describe its application. Projects must satisfy all applicable standards.

STANDARD	RATIONALE
<p>S2.1 MASSING: LOT SPLIT MAINTAIN 25' OF OPEN SPACE BETWEEN BUILDINGS</p>	
<p>SB-9 allows single-family zoned lots to be sub-divided so long as the resulting lot split does not result in any lot less than 1,200 square feet in size. In the case a lot split is proposed adequate space shall be provided for exposure, light and open space. Precedents in practice and the Planning Code have maintained 25' as a minimum to ensure the livability of the resulting units and neighboring buildings.</p>	
<ul style="list-style-type: none"> » A minimum 25' separation between the primary facades of the two facing structures shall be provided. » Architectural projections including balconies and bays into the shared court are allowed per Planning Code Section 136 for front yards and setbacks. 	<p>A 25' courtyard allows for quality shared open space.</p>
MEANS	EXAMPLE



STREET VIEW OF A SINGLE LOT DEVELOPMENT WITH THREE UNITS

S SITE DESIGN

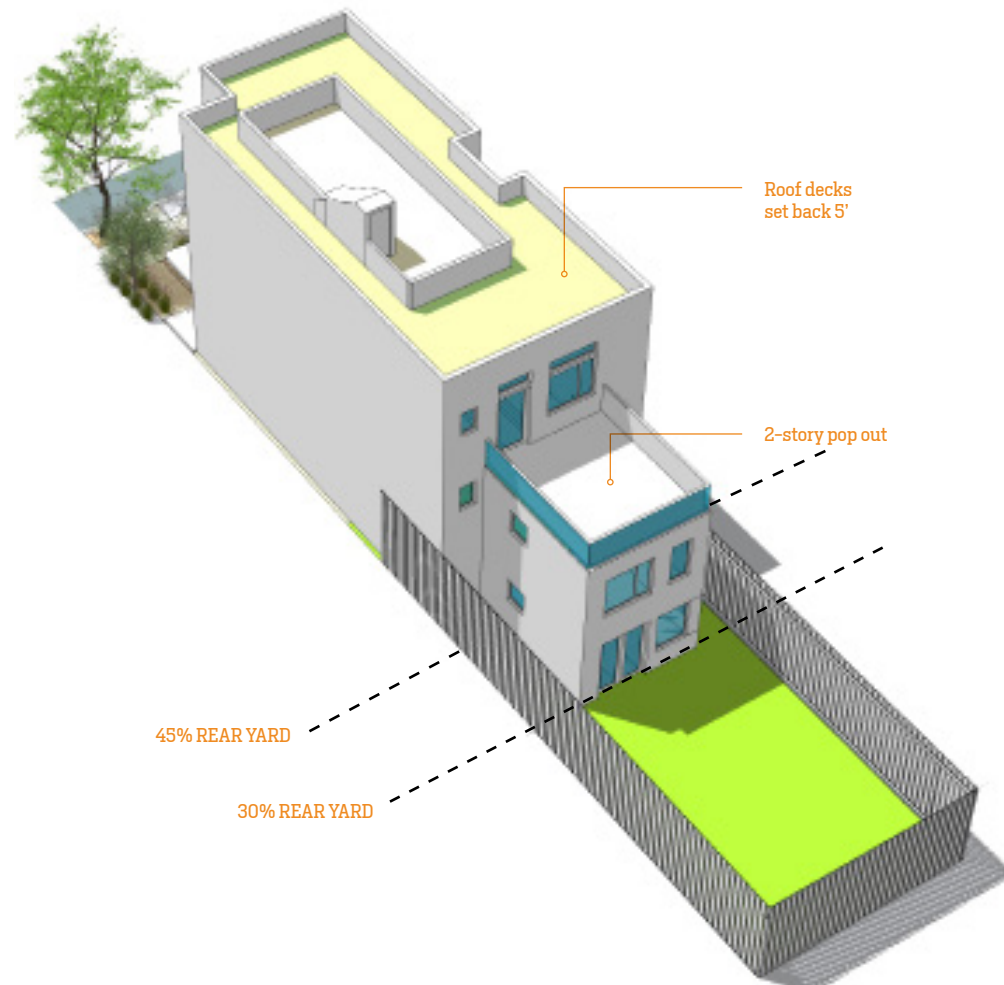
- S1 Massing: Single Lot
- S2 Massing: Lot Split
- S3 Match Lightwells and Side Setbacks
- S4 Roof Decks and Penthouses
- S5 Parking
- S6 Landscape

SI.1

MASSING: SINGLE LOT SCULPT BUILDING BETWEEN THE 45% AND 30% REAR YARD

The massing described easily accommodates a building volume that can provide two or three dwelling units with ample open space while maintaining reasonable access to light and mid-block open space to adjacent residents.

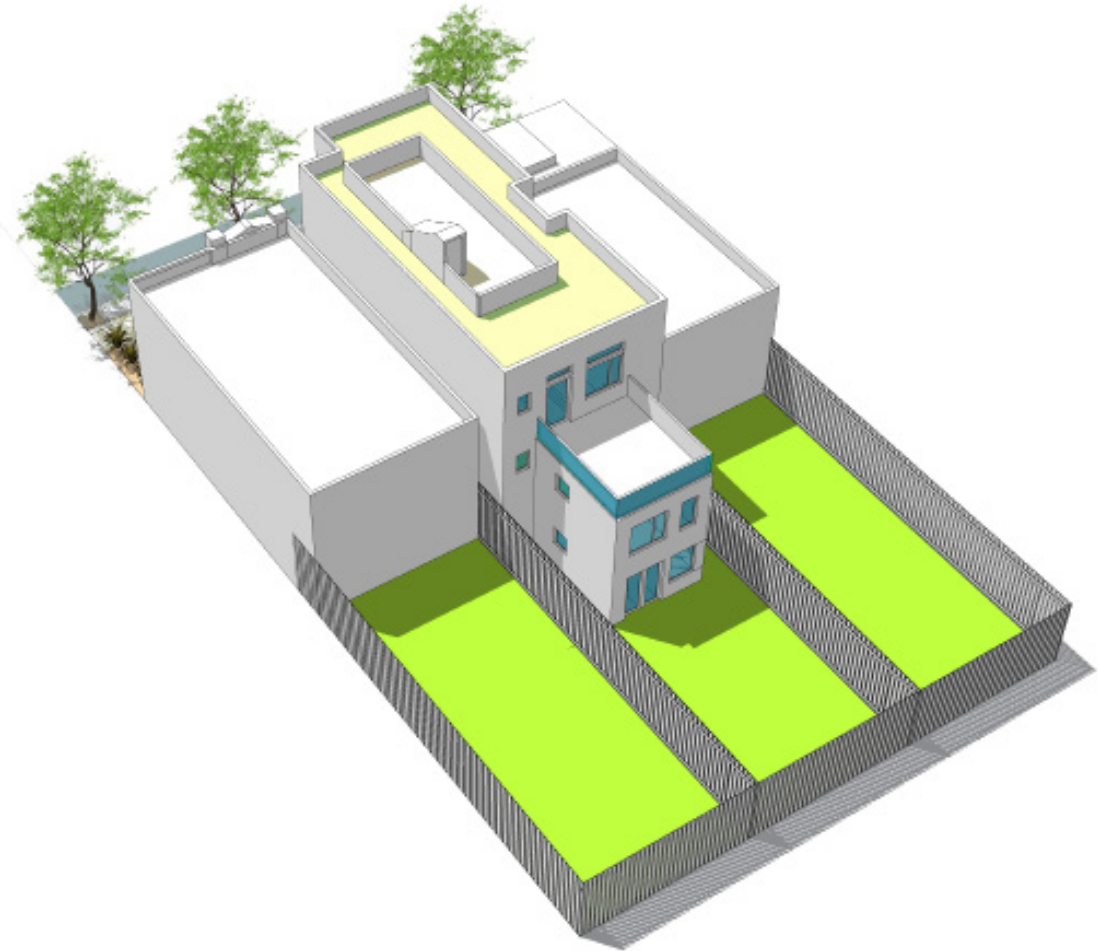
- » Limit the three-story volume to the 45% rear yard line. A two-story pop-out with 5-foot side setbacks may extend to the 30% rear yard line.
 - Basement space is permitted to extend the full width of the lot.
 - For corner lots, a setback is not required for the street-facing portion of the pop-out. A 5-foot side setback facing the interior lot line is required.
- » The rear 30% of the lot shall be unstructured at and below grade to allow for in-ground planting.



S1.2**UNIT SIZE PARITY**

Providing unit parity ensures that an extreme disparity between an oversized and undersized unit does not result from SB-9 .

- » To ensure reasonable parity between the proposed dwelling units the second unit within the same structure may not be less than 800 sqft.



Rear bird's eye view of three story duplex with ADU in context



Front bird's eye view of three story duplex with ADU in context

S2.1**MASSING: LOT SPLIT (FRONT TO BACK)
MAINTAIN 25' OF OPEN SPACE BETWEEN BUILDINGS**

SB-9 allows single-family zoned lots to be sub-divided so long as the resulting lot split does not result in any lot less than 1,200 square feet in size. In the case a lot split is proposed adequate space shall be provided for exposure, light and open space. Precedents in practice and the Planning Code have maintained 25' as a minimum to ensure the livability of the resulting units and neighboring buildings.

- » A minimum 25' separation between the primary facades of the two facing structures shall be provided.
- » Architectural projections including balconies and bays into the shared court are allowed per Planning Code Section 136 for front setbacks.

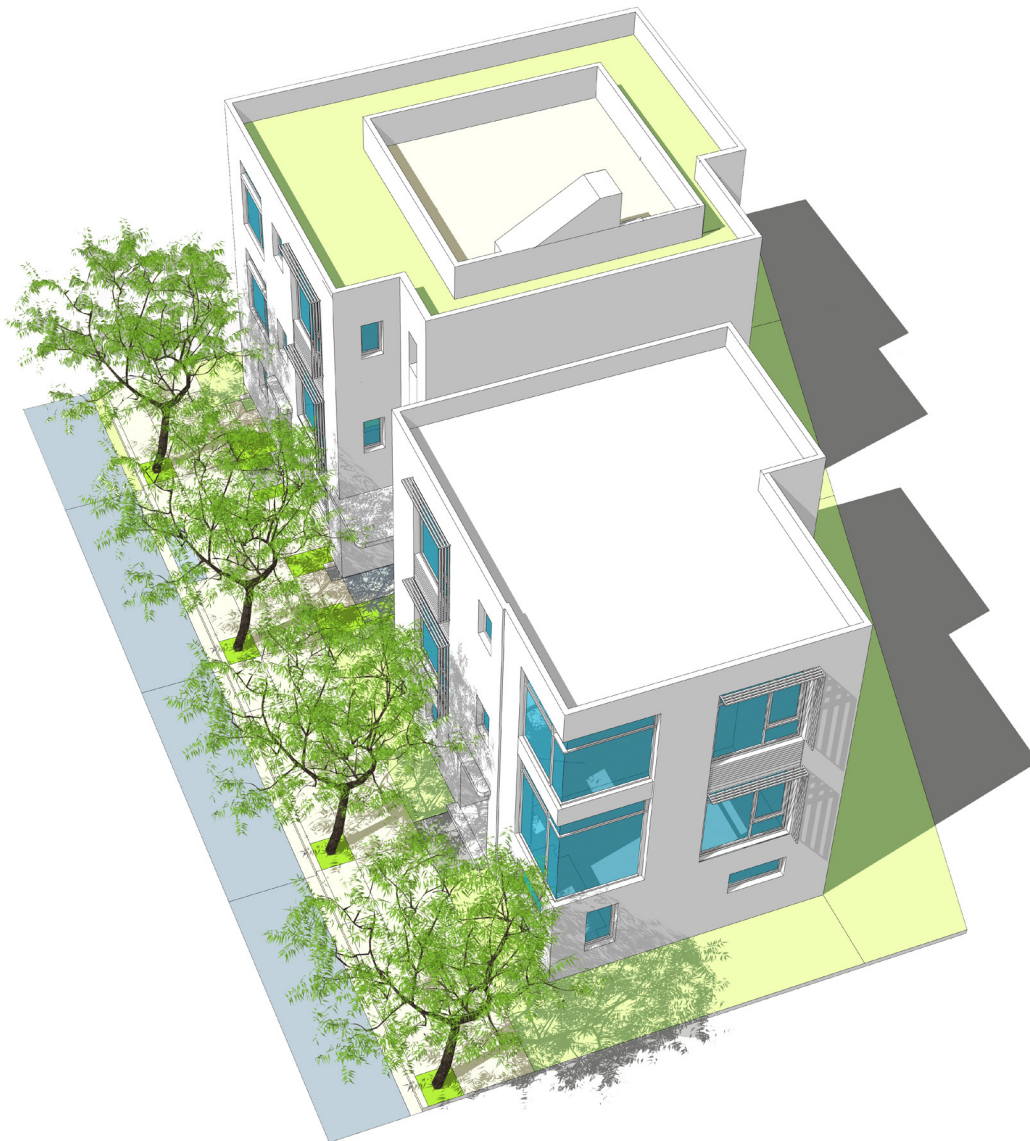


A 25' courtyard allows for quality shared open space.

S2.2**MASSING: LOT SPLIT (SIDE TO SIDE)**

A 50' wide lot split into two standard 25' by 100' lots should maintain massing at the front

S2.3 **MASSING: LOT SPLIT: (IRREGULAR LOT)**



An irregular lot split massing example

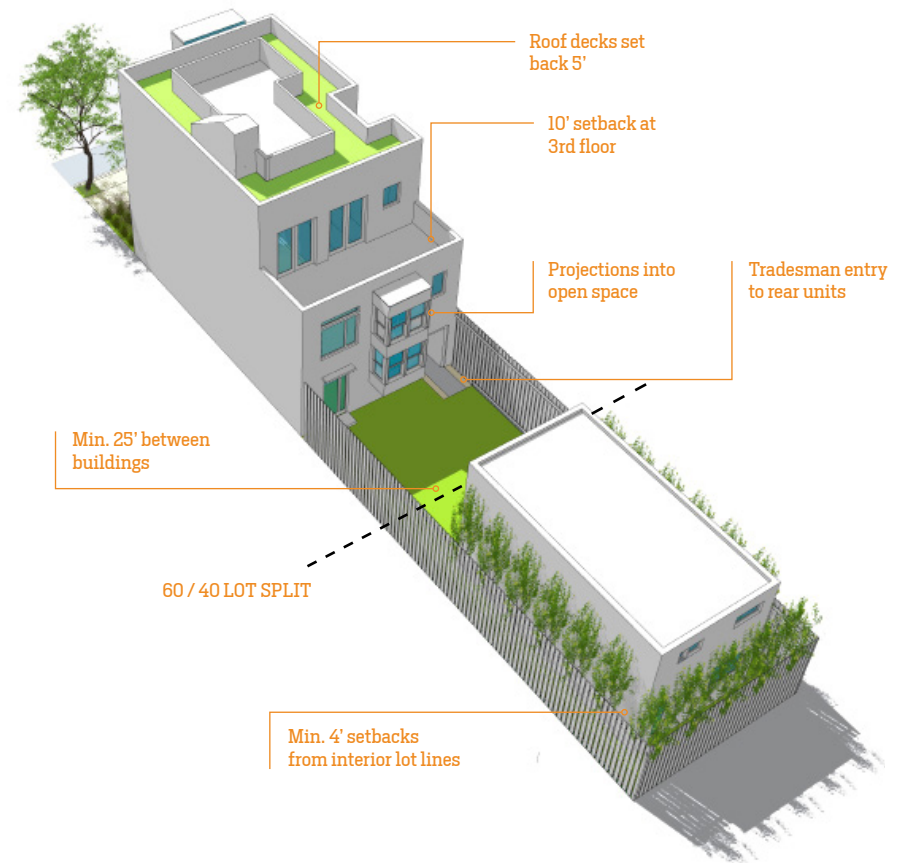
S2.4**LIMIT THE HEIGHT OF THE REAR BUILDING TO 20'**

Limiting the massing of rear buildings that do not have direct street frontage preserves light for the open space of the proposed new lots and the adjoining neighbors.

- » The rear building height shall be limited to 20' for all lots measured from grade using the methodology expressed in 260 (a) (1) (C) of the Planning Code.
- » No roof parapets higher than 6" are allowed.
- » Dormers are not allowed on rear buildings above the 20' height limit.



Limiting rear buildings to 20' in height minimizes impacts to neighbors.



Bird's eye view illustrating the primary lot split massing standards.

S2.5**STEP BACK THE UPPER FLOOR OF THE FRONT BUILDING FROM THE REAR**

Stepping back the upper floor in the rear increases exposure and usability of the common shared court. Respecting scale and proportion in the design of buildings help maintain continuity with the existing context.

- » When facing a structure in the rear, the street facing building shall set back a minimum of 10' on the upper floor for the full building width.



The third floor steps back 10' to provide relief to lower building in the rear.

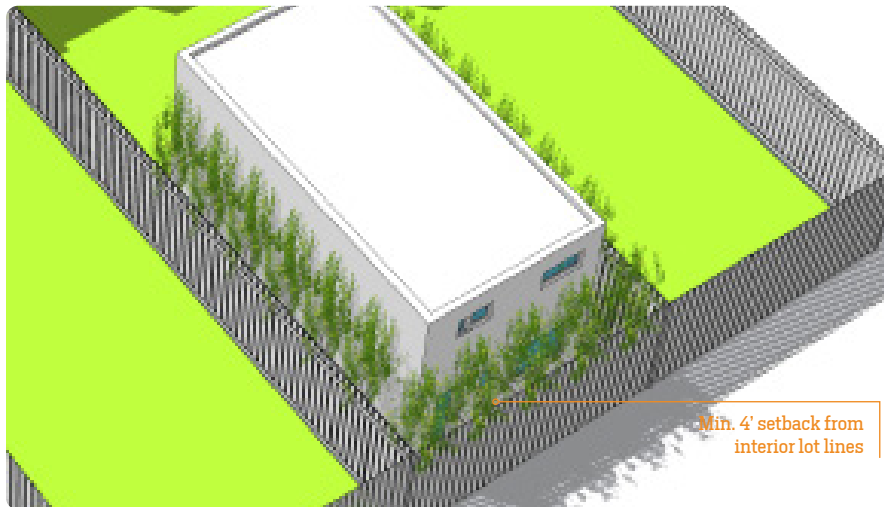


Front bird's eye view of lot split three story duplex with ADU in front and duplex in rear.

S2.6**SETBACK THE REAR BUILDING FROM THE SIDE AND REAR LOT LINES**

Providing a minimal side or rear yard allows for increased unit exposure and reduces privacy impacts to adjacent neighbors.

- » New construction must provide a minimum 4' setback from all interior lot lines for structures located at the rear of the property.
- » Corner properties are not required to have a rear or street facing setback. A side setback facing the internal neighbor is required.



A minimal setback from interior lot lines provides relief to neighbors and additional exposure.

S3.1**MATCH LIGHTWELLS AND SETBACKS**

Side setbacks and light-wells provide light and air to narrow and deep buildings. These qualities should be protected to ensure the future livability and adaptability of these buildings.

- » Match existing lightwells and side setbacks by providing light wells and side setbacks that match existing adjacent conditions by at least 75% of the length and are a minimum of 3' deep. Matching light wells must extend to the lowest portion of the existing building.

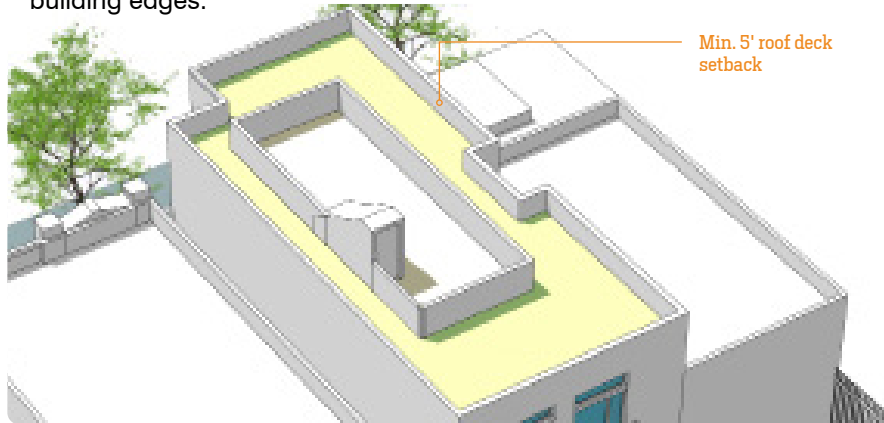


Match adjacent light wells to maximize light to both properties.

S4.1**SET BACK ROOF DECKS AND BALCONIES
5' FROM ALL BUILDING EDGES**

Roof decks by their location create conditions for privacy and noise impacts. Setbacks from building edges help buffer impacts to privacy, noise and light that potentially emanate from roof decks.

- » The occupied area of the roof decks and balconies shall be set back 5' from all building edges. Landscape may be placed in between edge of roof and occupied roof area.
- » Roof decks on the rear structure are not permitted.
- » Stair penthouses are not allowed on rear buildings.
- » Stair penthouses on the front building shall be set back 15' from front building wall; shall be limited to a single penthouse* (roof deck occupancy limit for a single means of egress); and shall be limited to the minimum dimensions (width, headroom height) required by the Building Code.
- » Use translucent or opaque guardrails.
- » Decks on a popout meeting S1.1 do not need to be set back from all building edges.



Setting back roof decks minimizes privacy impacts to neighbors.

S5.1**MINIMIZE PARKING**

In order to prioritize housing and encourage public transit parking will be minimized in SB-9 projects.

- » Parking shall be limited to one space per unit.
- » Maximum dimension of parking spaces shall not exceed requirements per Planning Code Section 154.



The parking garage is a secondary feature on the facade.

S6.1**LANDSCAPE SETBACKS**

Landscape reduces runoff, enhances privacy, and provides a soft transition from the public to private realm.

- » Separate driveway from pedestrian paths by a landscaping strip not less than 18”.



Landscaping helps identify the main entry.



Landscaping in the front setback provides a soft transition from the public to private realm.



Separating the tradesman access to rear units by a landscape strip identifies it as a pedestrian entry.



INCREASED LANDSCAPE REQUIREMENTS HELP INTEGRATE DENSER HOUSING INTO EXISTING NEIGHBORHOODS.



STREET VIEW OF A LOT SPLIT DEVELOPMENT IN CONTEXT OF EXISTING DEVELOPMENT.



ARCHITECTURE

A1 Materials

A2 Windows

A3 Entries

A1.2**USE SUSTAINABLE, DURABLE MATERIALS**

Using durable, sustainable cladding materials will help with the integration of larger buildings.

- » The following exterior cladding materials shall not be allowed: Vinyl, plywood, any tropical hardwoods or tropical hardwood wood products as well as virgin redwood or virgin redwood wood products.

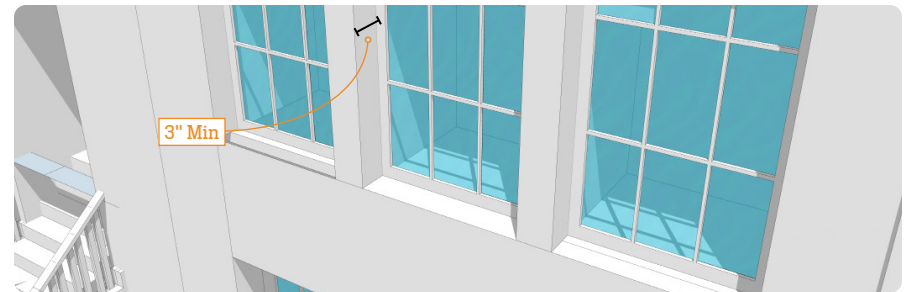


Durable, sustainable materials ensure that cladding will last.

A2.1**USE APPROPRIATE WINDOWS FOR FACADES VISIBLE FROM THE STREET**

The type, material, and detailing of windows is one of the single most important features on a building facade. Selecting and detailing appropriate windows is essential to support the character of neighborhoods throughout the city.

- » Windows for new construction shall provide a minimum of 3" shadow line for facades visible from the street. This may be achieved by one of the following means:
 - A window recess measured from the primary exterior cladding to sash
 - A projecting window measured from the primary cladding to the window sash
 - A sun screen surrounding all sides of the window
- » Detail(s) demonstrating shadow line requirements shall be included in plans.
- » Vinyl windows are not acceptable for facades visible from the street.



The window recess is measured from the sash to the primary cladding material.

A3.1**INCORPORATE PROMINENT ENTRIES**

Entries are the first impression of a residence and their design sets the tone for the entire building. As a transition from the public to private realm they must be given a high priority in the design approach.

- » Entries to multiple units on the street facing lot shall directly face the public right of way.
- » Entries for new construction must be raised a minimum of 3' above grade or recessed a minimum of 3' from the primary building wall.
- » Where ground floor units face a narrow street as described in Planning Code Section 261.1 a canopy projecting a minimum of 18" for the width of the entry may be used instead of a recessed or raised entry.



The width, recess and detailing of this entry identifies it as a primary facade feature.



This raised entry to upper units is the building's main architectural feature, identifying its importance.